



**Board of Adjustment**

**Staff Report  
BOA-24011**

**Hearing Date:** February 24, 2026  
**Prepared by:** Erin Roark  
eroark@cityoftulsa.org  
918-596-7618

**Owner and Applicant Information**

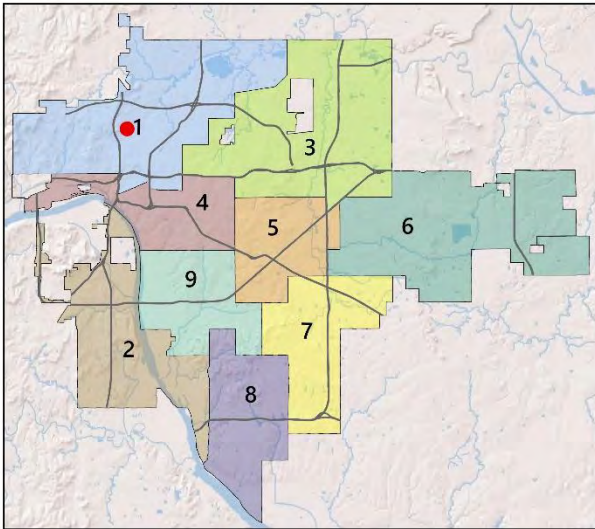
Applicant: RCJ Designs LLC  
Property Owner: Tito Properties LLC

**Property Location**

2224 North Martin Luther King Jr. Boulevard  
  
Tract Size: ±0.15 acres

**Location within the City of Tulsa**

*(shown with City Council districts)*



**Elected Representatives**

City Council: District 1, Vanessa Hall-Harper  
County Commission: District 1, Stan Sallee

**Public Notice Required**

Newspaper Notice – min. 10 days in advance  
Mailed Notice to 300’ radius – min. 10 days in advance

**Request Summary**

Variance to reduce the required 35-foot arterial street setback in the RS-3 district (Section 5.030-A, Table 5-3).

**Zoning**

Zoning District: RS-3  
Zoning Overlays: HNO

**Comprehensive Plan Considerations**

**Land Use**

Land Use Plan: Neighborhood  
Small Area Plans: Unity Heritage  
Development Era: Early Automobile

**Transportation**

Major Street & Highway Plan: Secondary Arterial  
planitulsa Street Type: Multi-Modal Corridor  
Transit: Regular Route  
Existing Bike/Ped Facilities: Sidewalk  
Planned Bike/Ped Facilities: Bike Lane, Sidewalks

**Environment**

Flood Area: N/A  
Tree Canopy Coverage: 0-9%  
Parks & Open Space: N/A

**Staff Analysis**

The applicant is requesting a variance to reduce the required 35-foot arterial street setback in the RS-3 district (Section 5.030-A, Table 5-3). Martin Luther King Jr. Boulevard is classified as a secondary arterial with a planned right-of-way width of 100 feet, or 50 feet from the centerline of the street. The applicant proposes an arterial street setback of 20 feet, 6 inches from the planned right-of-way. The applicant is requesting 14 feet, 6 inches of relief.

**Section 5.030 Lot and Building Regulations**

**5.030-A Table of Regulations**

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*Table 5-3: R District Lot and Building Regulations*

Regulations	RE	RS-1	RS-2	RS-3	RS-4	RS-5	RD	RT	RM-0	RM-1	RM-2	RM-3	RMH
<b>Min. Building Setbacks (ft.)</b>													
Street [3]													
Arterial or fwy service rd.	35	35	35	35	35	35	35	35	10	10	10	10	35

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**Section 90.090 Setbacks**

**90.090-A Measurement**

Required setbacks are measured from the applicable lot line, right-of-way, planned right-of-way or location referred to below. Building setbacks are measured to the nearest exterior building wall. Minimum setbacks that apply to other features (parking areas, fences, storage areas) are measured from the nearest point of the area or feature for which a setback is required. See [90.090-C](#) for information on structures and building features that are allowed to occupy setback and yard areas in R zoning districts. Unless otherwise expressly stated, no part of any structure may be located within the street right-of-way, nor within the planned right-of-way of streets shown on the major street and highway plan, nor within 25 feet of the centerline of the right-of-way on streets not shown on the major street and highway plan. If a variance of the prohibition against location of a structure within the right-of-way or planned right-of-way is granted by the Board of Adjustment, no part of any structure may be located within the street right-of-way, nor within the planned right-of-way of streets shown on the major street and highway plan, nor within 25 feet of the centerline of the right-of-way on streets not shown on the major street and highway plan, unless a license has been granted by the city, in the case of the right-of-way, or a removal agreement has been entered into, in the case of the planned right-of-way.

1. Street setbacks are measured from the actual right-of-way line of the street (other than an alley), provided that if the following measurement results in a greater setback, the greater setback applies:
  - a. For streets shown on the major street and highway plan, if the width of the planned right-of-way exceeds the width of the actual right-of-way, the measurement must be taken from the planned right-of-way; and
  - b. For streets not shown on the major street and highway plan, if the width of the right-of-way is 50 feet or less, the measurement must be taken from a point that is 25 feet from the centerline of the actual right-of-way.

Relevant Case History

- None found.

**Statement of Hardship**

The applicant's statement of hardship is included as a separate exhibit.

**Comprehensive Plan Considerations****Land Use Plan**

The subject property is designated as Neighborhood. Neighborhoods are mostly residential uses, which includes detached, missing middle, and multi-dwelling unit housing types. Churches, schools, and other low intensity uses that support residents' daily needs are often acceptable, particularly for properties abutting Multiple Use, Local Center, or Regional Center land use areas. Multi-dwelling unit housing that takes access off an arterial is considered Multiple Use, Local Center, or Regional Center. If a multi-dwelling unit housing property takes access from a lower-order street separated from the arterial, then it would be considered Neighborhood.

Surrounding Properties:

<u>Location</u>	<u>Existing Zoning/Overlay</u>	<u>Existing Land Use Designation</u>	<u>Existing Use</u>
North	RS-3/HNO	Neighborhood	Residential
East	RS-3/HNO	Neighborhood	Residential
South	RS-3/HNO	Neighborhood	Residential
West	RS-3/HNO	Neighborhood	Residential

**Small Area Plans**

The subject property is included in the Unity Heritage Neighborhoods Plan. The eight defined goals of this plan, established in 2016, are as follows:

1. Enhance the desirability of all neighborhoods in the planning area.
2. Preserve and stabilize the area's healthy neighborhoods.
3. Transform and revitalize neighborhoods most impacted by vacancy or poor maintenance.
4. Capitalize on OSU-Tulsa, Langston University Tulsa, and proximity to Downtown to spur redevelopment of the southern edge of the Unity Heritage area.
5. Build on the presence of industry and employment to add additional jobs and employ local residents.
6. Celebrate the area's history and strengthen its character.
7. Enhance local commerce and access to important goods and services.
8. Expand multi-modal transportation options throughout the Unity Neighborhoods area.

**Development Era**

The subject property is in an area developed during the Early Automobile Era (1930s-50s), which retained a high degree of connectivity from neighborhood streets to the arterial network, with mostly commercial, office, industrial, and other active uses along major streets and a mix of housing options and neighborhood-based uses like schools, churches, and libraries in the interior sections. Priorities in these areas include walkability, bikeability, access to public transit, historic preservation, housing type variety, mixed-use development, commercial revitalization, compatibility of scale for neighborhood development, and transitions between commercial corridors and residential areas.

**Transportation**

Major Street & Highway Plan: Martin Luther King Junior Boulevard runs parallel to the east of the subject property and is classified as a Secondary Arterial, which has a planned minimum right-of-way width of 100 feet.

Comprehensive Plan Street Designation: Martin Luther King Junior Boulevard is designated as a Multi-Modal Corridor. Multi-modal streets support commercial and residential development along major arterial streets. These streets align

with the recommendations for on-street bicycle infrastructure established in the 2015 GO Plan, and they should be evaluated for feasibility with regard to the reallocation of street space for bicycle facilities.

Transit: Regular Route 110 runs along Martin Luther King Junior Boulevard.

Existing Bike/Ped Facilities: Sidewalks are present along Martin Luther King Junior Boulevard.

Planned Bike/Ped Facilities: The 2015 GO Plan proposes a Bike Lane is along Martin Luther King Junior Boulevard. Sidewalks are recommended along all street frontages.

Arterial Traffic per Lane: Martin Luther King Junior Boulevard has an average annual daily traffic (AADT) count of 1,993 vehicles per lane.

**Environmental Considerations**

Flood Area: The subject property is not located within a designated flood area.

Tree Canopy Coverage: Tree canopy in the area is 9%. Preserving the limited existing canopy should be encouraged, as well as measures to increase the canopy through landscaping. Street-lining trees in particular should be encouraged to spread the benefit of the tree canopy to the pedestrian realm.

Parks & Open Space: N/A

**Site Photos**



View west from Martin Luther King Junior Boulevard, December 2025 (Image from Google Street View)

**Sample Motion**

I move to approve or deny a variance to reduce the required arterial street setback in the RS-3 district from 35 feet to 20 feet, 6 inches (Section 5.030-A, Table 5-3);

- per the conceptual plan(s) shown on page(s) \_\_\_\_\_ of the agenda packet.
- subject to the following conditions (including time limitation, if any): \_\_\_\_\_.

The board finds the hardship to be \_\_\_\_\_.

In granting the Variance, the Board finds that the following facts, favorable to the property owner, have been established:

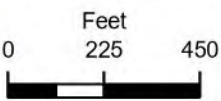
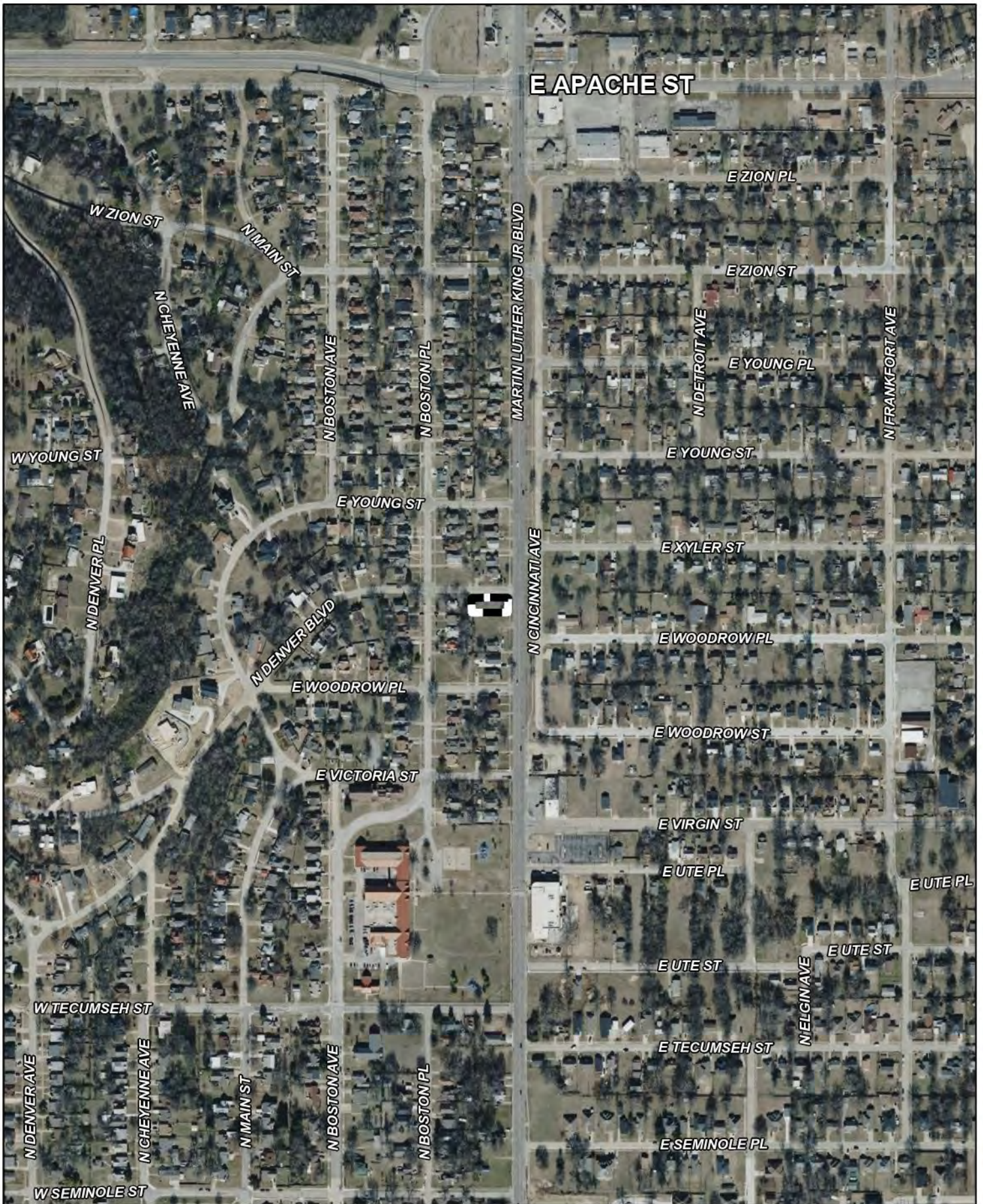
- That the physical surroundings, shape, or topographical conditions of the subject property would result in unnecessary hardships or practical difficulties for the property owner, as distinguished from a mere inconvenience, if the strict letter of the regulations were carried out;
- That literal enforcement of the subject zoning code provision is not necessary to achieve the provision’s intended purpose;
- That the conditions leading to the need of the requested variance are unique to the subject property and not applicable, generally, to other property within the same zoning classification;
- That the alleged practical difficulty or unnecessary hardship was not created or self-imposed by the current property owner;
- That the variance to be granted is the minimum variance that will afford relief;
- That the variance to be granted will not alter the essential character of the neighborhood in which the subject property is located, nor substantially or permanently impair use or development of adjacent property; and
- That the variance to be granted will not cause substantial detriment to the public good or impair the purposes, spirit, and intent of this zoning code or the comprehensive plan.

**Property Description**

LOT 1, BLOCK 10, OAK CLIFF ADDN, City of Tulsa, Tulsa County, State of Oklahoma

**Exhibits**

- Case map
- Aerial (small scale)
- Aerial (large scale)
- Tulsa Comprehensive Plan Land Use Map



Subject  
Tract

**BOA-24011**

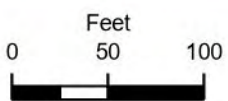
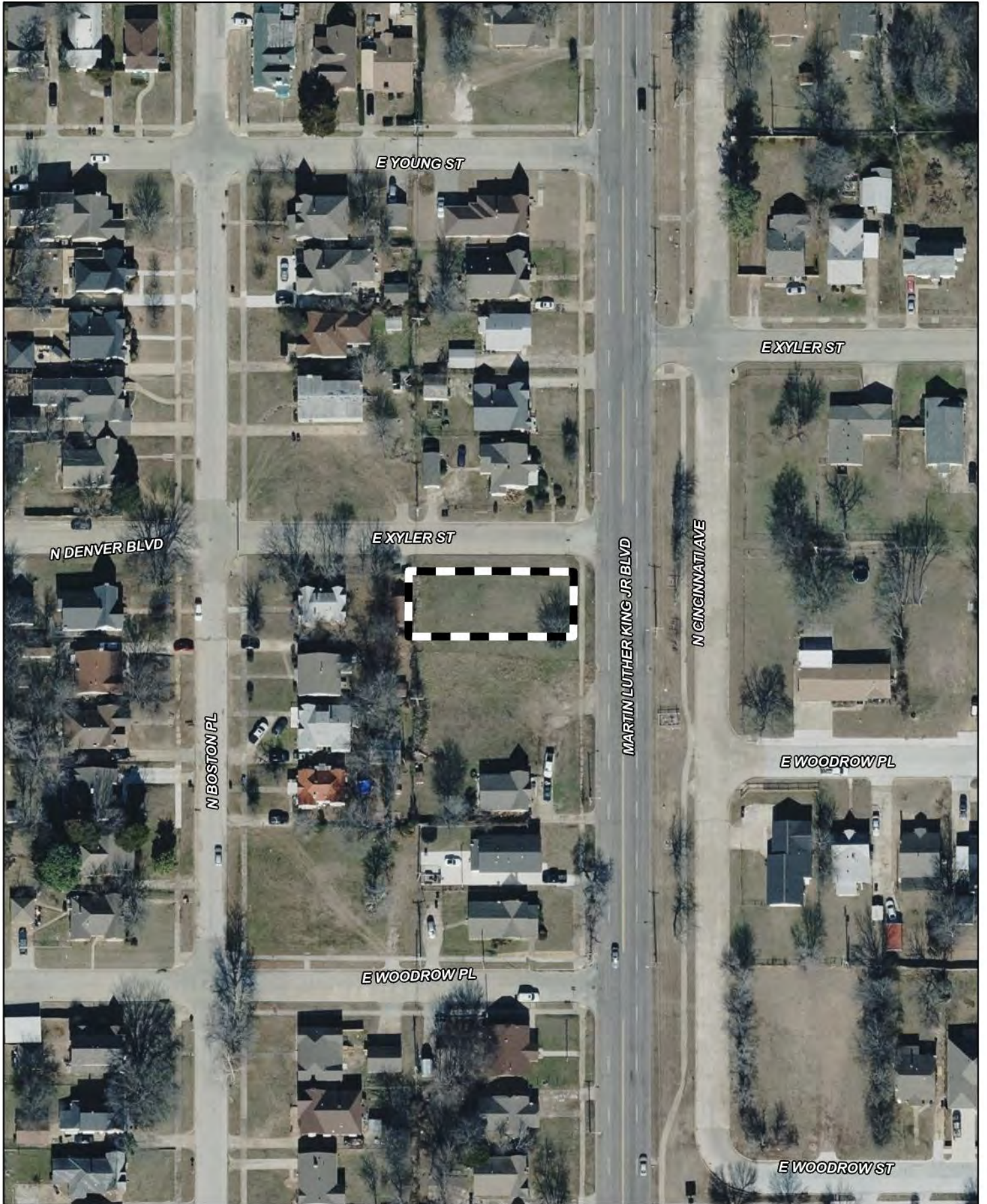
20-12 26

Note: Graphic overlays may not precisely align with physical features on the ground.

Aerial Photo Date: 2025



10.6



Subject  
Tract

**BOA-24011**

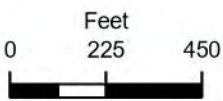
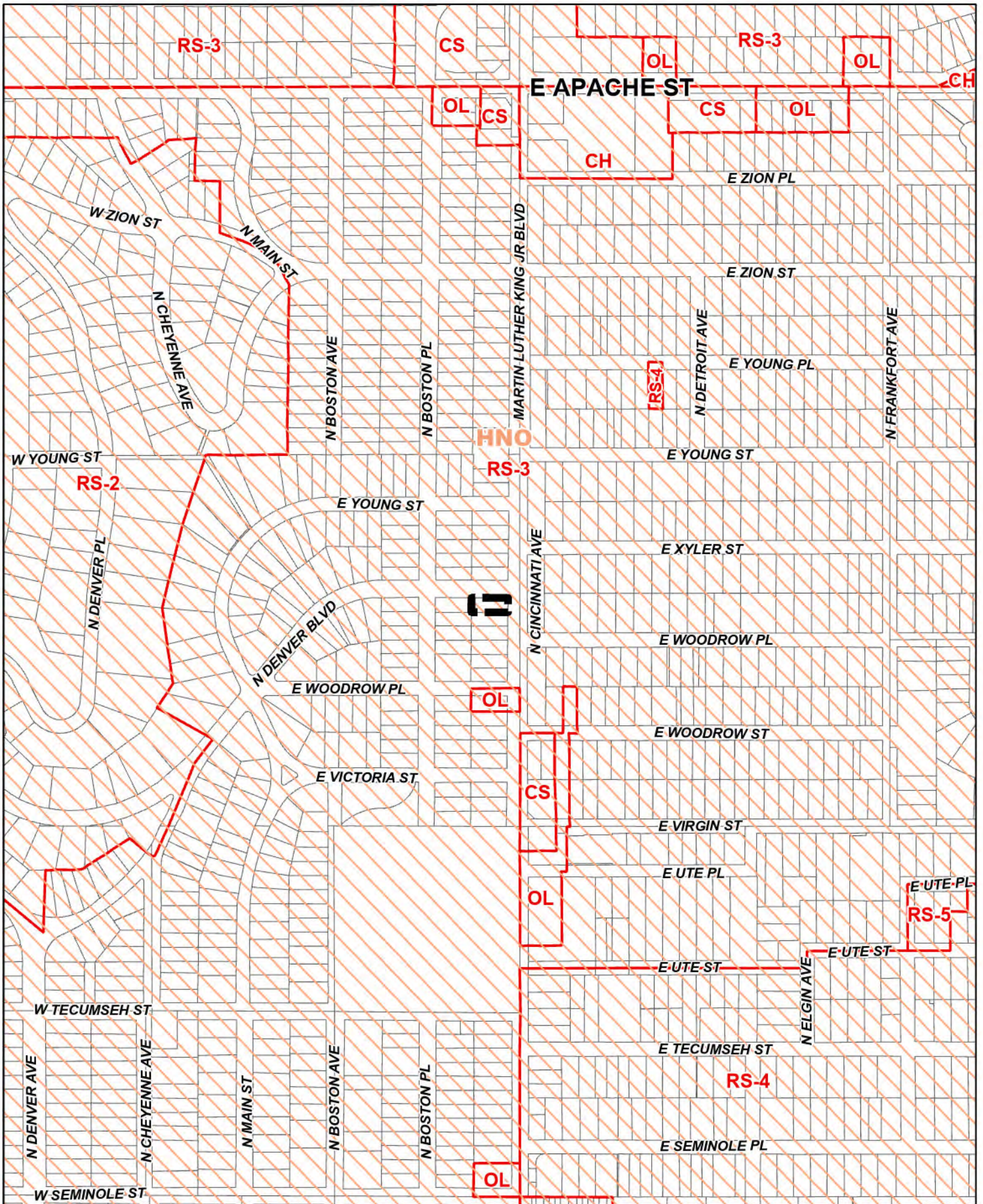
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Note: Graphic overlays may not precisely align with physical features on the ground.

Aerial Photo Date: 2025



10.7



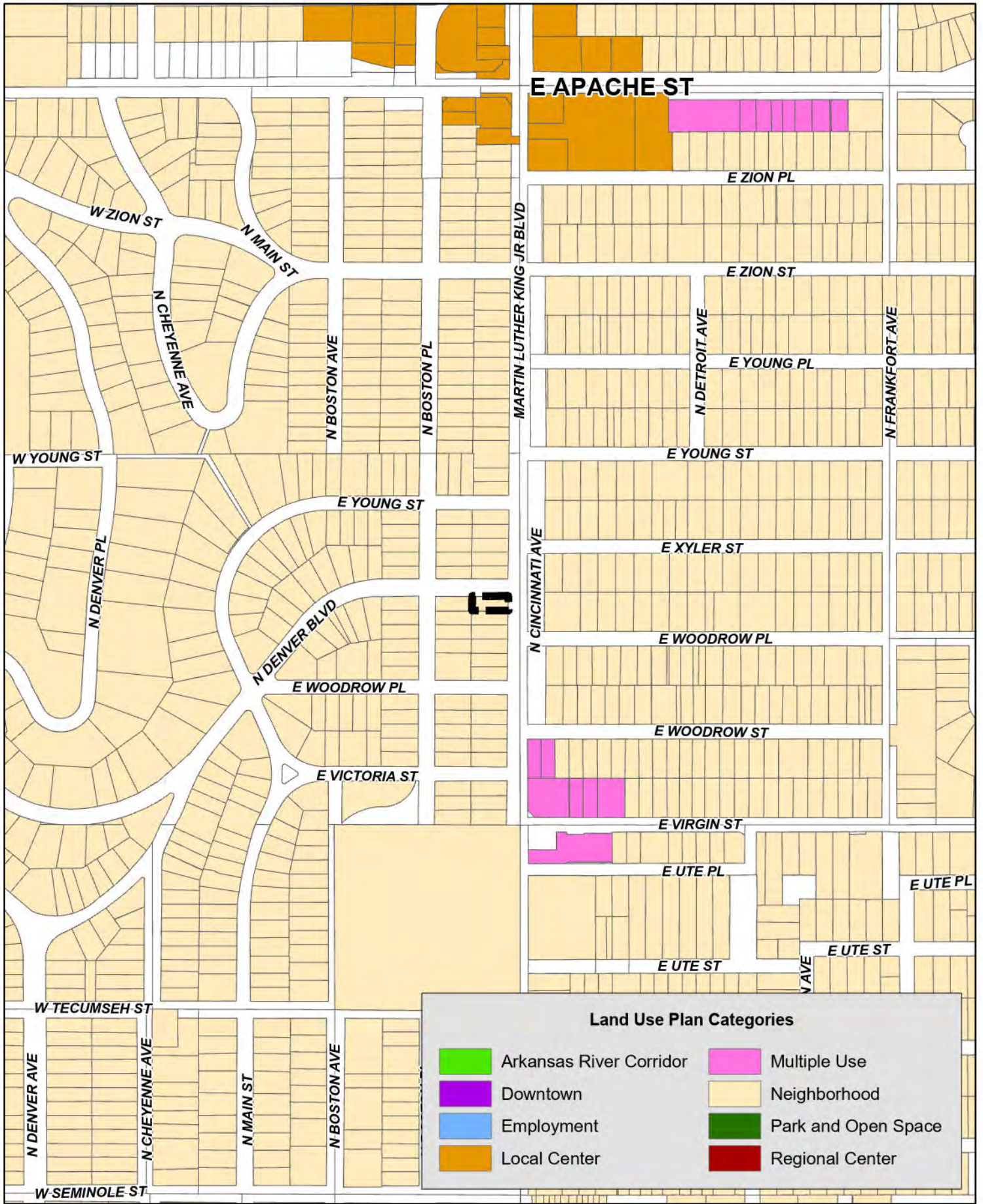
 Subject Tract

**BOA-24011**









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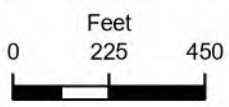


10.8



**Land Use Plan Categories**

 Arkansas River Corridor	 Multiple Use
 Downtown	 Neighborhood
 Employment	 Park and Open Space
 Local Center	 Regional Center



 Subject Tract

**BOA-24011**

20-12 26



10.9

## Variances

### Proof of Hardship

The Board of Adjustment is allowed to approve variances only after determining that the following conditions exist. Below, describe how your request satisfies **each** of these conditions.

1. That the physical surroundings, shape, or topographic conditions of the subject property would result in unnecessary hardships or practical difficulties for the property owner, as distinguished from a mere inconvenience, if the strict letter of the regulations were carried out;

Property is uniquely impacted by the Major Street and Highway Plan designation of N. Martin Luther King Jr. Blvd. as a Secondary Arterial with a planned right-of-way width of 100 feet. The requirement to measure the front setback from the planned right-of-way line, rather than the existing right-of-way, results in an unusually deep effective setback of 85 feet from the centerline of the street. This significantly reduces the buildable area of the lot and creates a practical difficulty that is not a mere inconvenience, but a condition arising from regulatory and physical constraints specific to this property's frontage on an arterial street.

2. That the literal enforcement of the subject zoning code provision is not necessary to achieve the provision's intended purpose;

The intent of the front setback requirement is to ensure adequate separation between structures and arterial streets for safety, circulation, and future roadway expansion. In this case, strict enforcement of the full planned right-of-way setback is not necessary to achieve that purpose, as the existing right-of-way has not been expanded and there is no immediate roadway improvement planned. A reduced setback would still maintain appropriate spacing, visibility, and compatibility with the street while allowing reasonable use of the property.

3. That the conditions leading to the need of the requested variance are unique to the subject property and not applicable, generally, to other property within the same zoning classification;

This hardship is unique to the subject property due to its direct frontage on a designated arterial street with a planned right-of-way that exceeds the existing right-of-way width. Other properties within the same RS-3 zoning district that do not front an arterial street, or that are not affected by planned right-of-way expansion, are not subject to this increased setback requirement. As such, the condition is not generally applicable to other properties in the zoning district.

4. That the alleged practical difficulty or unnecessary hardship was not created or self-imposed by the current property owner;

The hardship was not created or self-imposed by the current property owner. The arterial street classification and planned right-of-way width were established through the City's Major Street and Highway Plan and exist independently of any actions taken by the owner. The request for relief arises solely from these pre-existing regulatory conditions.

5. That the variance to be granted is the *minimum* variance that will afford relief;

The requested variance represents the minimum relief necessary to allow reasonable development of the property. The proposal seeks only to reduce the front setback measured from the planned right-of-way to a distance that reflects the existing right-of-way conditions, while still maintaining adequate separation from the street. No additional zoning relief beyond what is necessary to address this specific hardship is being requested.

6. That the variance to be granted will not alter the essential character of the neighborhood in which the subject property is located, nor substantially or permanently impair use or development of adjacent property; and

Granting the requested variance will not alter the essential character of the neighborhood or impair the use or development of adjacent properties. The proposed development will remain consistent with surrounding residential development patterns and setbacks along N. Martin Luther King Jr. Blvd. The reduced setback will not introduce any use, scale, or intensity that is incompatible with the surrounding area.

7. That the variance to be granted will not cause substantial detriment to the public good or impair the purposes, spirit, and intent of the zoning code or the comprehensive plan.

The requested variance will not be detrimental to the public good nor impair the purposes, spirit, or intent of the zoning code or the comprehensive plan. Allowing reasonable relief from the planned right-of-way setback promotes practical land use while maintaining public safety, neighborhood compatibility, and long-term planning objectives.

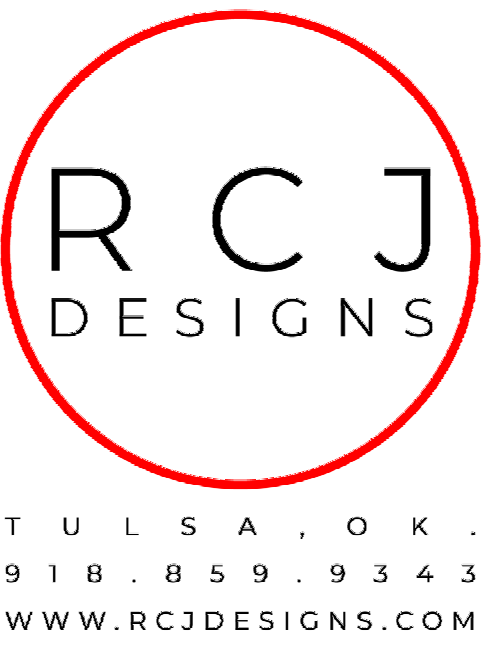
# TITO PROPERTIES

## NEW BUILD

2224 M.L.K. JR BLVD  
TULSA, OK 74106

**PROJECT NUMBER**  
25173

**PERMIT SET**  
12.17.2025



**PROJECT ISSUE**  
PERMIT SET

**ISSUE DATE**  
12.17.2025

**PROJECT ADDRESS**  
2224 M.L.K. JR BLVD  
TULSA, OK 74106

**PROJECT NUMBER**  
25173



FOR REFERENCE PURPOSES ONLY

### PROJECT TEAM

**DESIGN:**  
RCJ DESIGNS, LLC  
8246 E 73RD ST  
TULSA, OK. 74133

RAUL CISNEROS JR  
918.859.9343  
RAUL@RCJDESIGNS.COM  
WWW.RCJDESIGNS.COM

### SCOPE OF WORK

**SUMMARY:**  
NEW BUILD ON VACANT LOT

### SHEET INDEX

A00	COVER SHEET
A0	SITE PLAN
A1	FLOOR PLAN
A2	EXTERIOR ELEVATIONS/ WALL SECTION
A3	EXTERIOR ELEVATIONS
A4	ROOF PLAN
A5	WALL SECTION/ DETAILS

# TITO PROPERTIES

## NEW BUILD

**LEGEND** SITE PLAN

NOT ALL ITEMS MAY BE USED

EXISTING ASPHALT PAVING

NEW ASPHALT PAVING/ TRACK

TULSA REGULATORY FLOOD PLAIN

NEW CONCRETE DRIVEWAY/WALKWAY

PROPERTY LINE

BUILDING SETBACKS

BUILDING OUTLINE

SILT FENCE

PRE-CONSTRUCTION DRAINAGE FLOW

POST-CONSTRUCTION DRAINAGE FLOW

STRAW WATTLE

DOWNSPOUTS

**PROJECT INFORMATION**

**LEGAL DESCRIPTION**

SUBDIVISION: OAK CLIFF ADDN (29775)  
 LEGAL: LT 1 BLK 10  
 SECTION: 26 TOWNSHIP: 20 RANGE: 12

**GENERAL INFORMATION**

ZONING: RS-3  
 LOT SIZE: 0.15 ACRES / 6,500 SQ FT  
 TOTAL HEATED AREA: 1,306 SQ FT

**ADDITIONAL INFORMATION**

PROPOSED LIVABLE AREA: 1,306 SQ FT  
 FRONT COVERED PORCH: 99 SQ FT  
 REAR COVERED PORCH: 140 SQ FT  
 PROPOSED DRIVEWAY: 609 SQ FT

**GRAND TOTAL: 2,154 SQ FT**

**REGULATIONS**

MIN. OPEN SPACE REQ'D: 4,000 SQ FT  
 OPEN SPACE PROPOSED: 4,346 SQ FT

MAX BUILDING HEIGHT: 35' - 0"  
 RES. STRUCTURE HEIGHT: +/- 20' 2"

**KEYNOTES** SITE PLAN

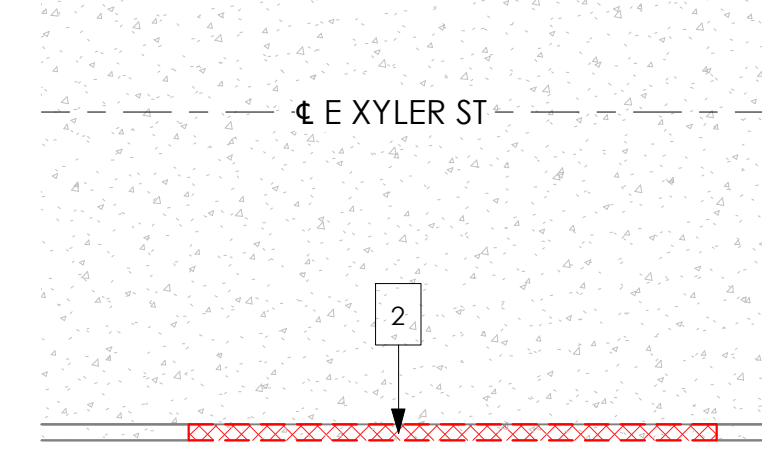
- DRIVEWAY & SIDEWALK, REFERENCE CITY OF TULSA STANDARD 701 DRIVEWAY/SIDEWALK DETAILS, DRIVEWAY & SIDEWALK REQUIRE SEPARATE REVIEW AND PERMITS.
- REMOVE EXISTING CURB/ PAVING BY SAWING

**SURVEY DISCLAIMER** SITE PLAN

- THIS SITE PLAN IS DRAWN WITHOUT THE BENEFIT OF A BOUNDARY SURVEY. BOUNDARY AND EXISTING STRUCTURE LOCATIONS ARE APPROXIMATE BASED ON FIELD INFORMATION AND FOR ILLUSTRATIVE PURPOSES ONLY.

**SIDEWALKS/CURB RAMPS/CROSSWALK NOTE**

- THE OWNER/CONTRACTOR SHALL VERIFY THAT THE EXISTING AND PROPOSED SIDEWALKS, CURB RAMPS AND CROSSWALKS ABUTTING PROPERTY COMPLY WITH THE AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES AND PROPOSED ACCESSIBILITY GUIDELINES FOR PEDESTRIAN FACILITIES IN THE PUBLIC RIGHTS OF WAY PUBLISHED ON JULY 26, 2011 BY THE US ACCESS BOARD. IF ANY EXISTING SIDEWALKS, CURB RAMPS AND CROSSWALKS ABUTTING THEIR PROJECT ARE FOUND TO NOT COMPLY WITH THE AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES THEN IN ACCORDANCE WITH TRO TITLE 35, CHAPTER 6, SECTION 601, PARAGRAPH F THE OWNER/CONTRACTOR SHALL REPAIR OR REPLACE THE EXISTING SIDEWALKS, CURB RAMPS AND CROSS WALKS AND BRING THEM INTO ADA COMPLIANCE AT THEIR OWN EXPENSE. DRIVEWAYS AND SIDEWALKS REQUIRE SEPARATE REVIEW AND PERMITS THRU THE RIGHT OF WAY DEPARTMENT.



**2 DEMO SITE PLAN**  
 1/8" = 1'-0"

**SILT FENCE DETAIL** SITE PLAN

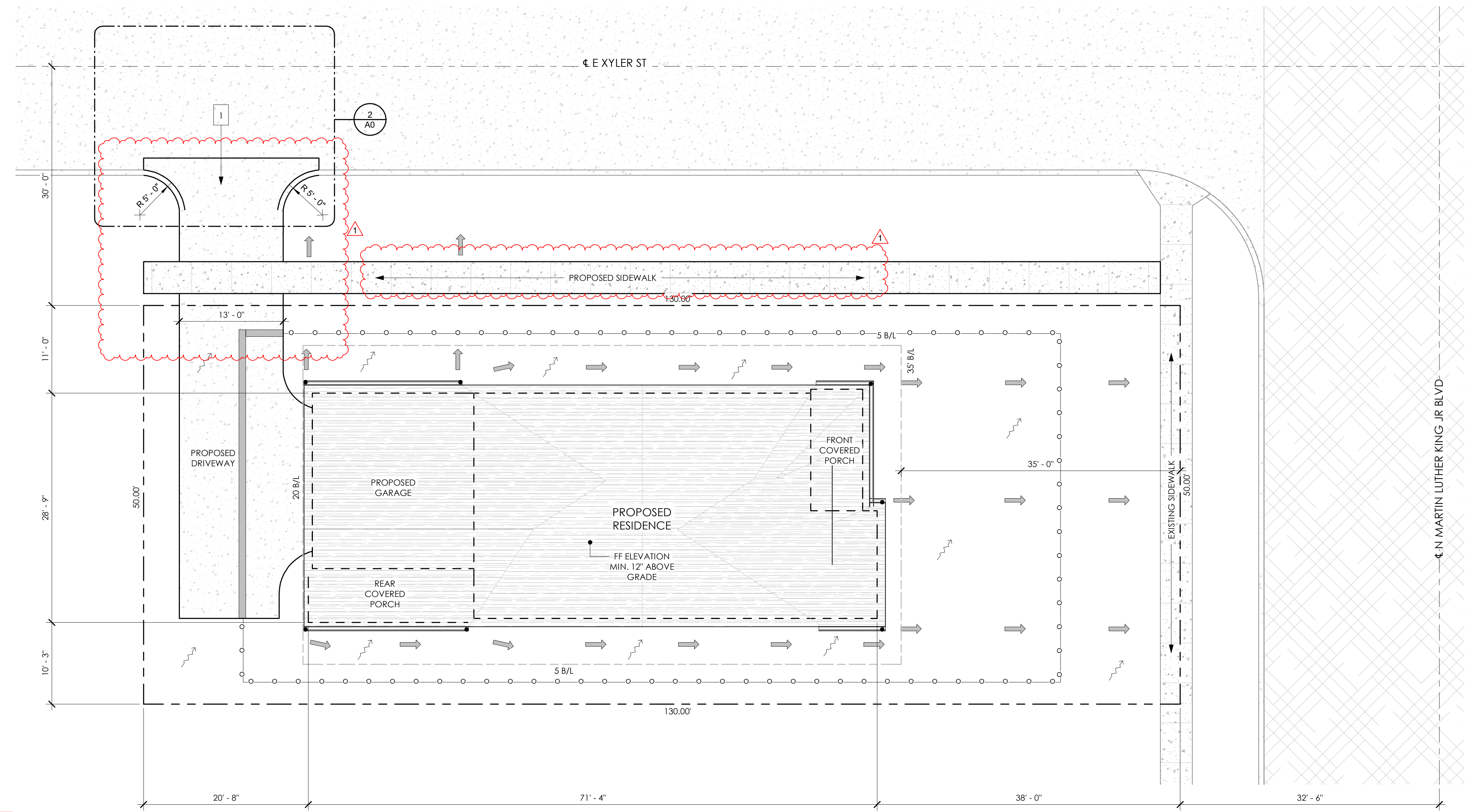
- STEEL POSTS WHICH SUPPORT THE SILT FENCE SHALL BE INSTALLED ON A SLIGHT ANGLE TOWARD THE ANTICIPATED RUNOFF SOURCE.
- THE TOE OF THE SILT FENCE SHALL BE TRENCHED IN WITH A SPADE OR MECHANICAL TENCHER, SO THAT THE DOWNSLOPE FACE OF THE TRENCH IS FLAT AND PERPENDICULAR TO THE LINE OF FLOW.
- THE TRENCH SHOULD BE A MINIMUM OF 6 INCHES DEEP AND 3 TO 4 INCHES WIDE TO ALLOW FOR THE SILT FENCE TO BE LAID IN THE TRENCH 4 INCHES AND BACKFILLED.
- SILT FENCE SHOULD BE SECURELY FASTENED TO EACH STEEL SUPPORT POST OR TO WOVEN WIRE, WHICH IS IN TURN ATTACHED TO THE STEEL FENCE POSTS.
- INSPECTION SHALL BE FREQUENT AND REPAIR OR REPLACEMENT SHALL BE MADE PROMPTLY AS NEEDED.
- SILT FENCE SHALL BE REMOVED WHEN IT HAS SERVED ITS USEFULNESS SO AS NOT TO BLOCK OR IMPEDE STORM FLOW OR DRAINAGE.
- SEDIMENT TRAPPED BY THIS PRACTICE SHALL BE DISPOSED OF IN AN APPROVED SITE, IN A MANNER THAT WILL NOT CONTRIBUTE TO ADDITIONAL SILTATION. ACCUMULATED SILT SHALL BE REMOVED WHEN IT REACHES A DEPTH OF 6 INCHES AND DISPOSED OF IN AN APPROVED SPOIL SITE OR AS IN NO. 7 ABOVE.

SCALE: NOT TO SCALE

**BEFORE YOU DIG. CAUTION**

NOTICE TO CONTRACTOR  
 THE CONTRACTOR IS SPECIFICALLY CAUTIONED THE LOCATION AND ELEVATION OF EXISTING UTILITIES AS SHOWN ON THESE PLANS ARE BASED ON RECORDS OF THE VARIOUS UTILITY COMPANIES AND MEASUREMENTS TAKEN IN THE FIELD. THE INFORMATION IS NOT TO BE RELIED ON AS BEING EXACT OR COMPLETE. THE CONTRACTOR MUST CALL THE LOCAL UTILITY LOCATION CENTER AT LEAST 72 HOURS BEFORE ANY EXCAVATION TO REQUEST EXACT FIELD LOCATIONS OF THE UTILITIES.

CALL OKIE  
 1-800-922-6543



**1 SITE PLAN**  
 1/8" = 1'-0"

**RCJ DESIGNS**

TULSA, OK  
 918.859.9343  
 WWW.RCJDESIGNS.COM

DRAWN FOR:  
**TITO PROPERTIES**

**NEW BUILD**

2224 M.L.K. JR BLVD  
 TULSA, OK 74106

NO.	REVISION	DATE
1	LOD #1	10.07.2025

PROJECT NUMBER:  
 25173

ISSUE:  
 PERMIT SET

DATE:  
 12.17.2025

DRAWN BY:  
 RCJ

CHECKED BY:  
 RCJ

SHEET TITLE:

**SITE PLAN**

**A0**

WALL ASSEMBLIES		FLOOR PLAN
ITEM	DESCRIPTION	
	NEW EXTERIOR WALL RE: EXTERIOR ELEVATIONS FOR FINISH	
	NEW INTERIOR WALL	

NOTE: ALL INTERIOR WALLS TO BE A4 U.N.O.

- PROVIDE WATER RESISTANT GYPSUM BOARD AT ALL WET AREAS.

GENERAL NOTES		FLOOR PLAN
1.	ALL INTERIOR WALLS TO BE A4 U.N.O.	
2.	ALL DIMENSIONS ARE TO FACE OF EXISTING WALLS AND CENTERLINE OF NEW WALL/ROUGH OPENING U.N.O.	
3.	ALL INTERIOR FINISHES INCLUDING BUT NOT LIMITED TO FLOORING, PAINT, WALLCOVERING, LIGHT FIXTURES, TRIM, LAMINATE, SOLID SURFACES AND MILLWORK TO BE SELECTED BY OWNER AND INSTALLED BY CONTRACTOR PER MANUF. GUIDELINES AND IN ACCORDANCE WITH ALL LOCAL CODES AND ORDINANCES.	
4.	GENERAL CONTRACTOR SHALL VISIT SITE AND BECOME FAMILIAR WITH SITE CONDITIONS.	
5.	NOTIFY DESIGNER IMMEDIATELY IF DIFFERENT CONDITIONS EXIST FROM WHAT IS DESCRIBED.	
6.	ALIGN NEW WALL'S FINISH FACE WITH THE EXISTING WALL'S FINISH FACE AT AREAS WHERE THE TWO ABUT.	
7.	ALL MECHANICAL, ELECTRICAL AND PLUMBING ITEMS TO BE PROVIDED ON A DESIGN/BUILD BASIS, IN ACCORDANCE WITH ALL LOCAL CODES AND ORDINANCES, ASIDE FROM ALL ELECTRICAL AND PLUMBING ITEMS REPRESENTED ON PLANS, PROVIDE ADDITIONAL DEVICES AS REQUIRED BY LOCAL CODE.	
8.	ALL DIMENSIONS ARE TO CENTERLINE OF OBJECT, EXCEPT FOR THE PERIMETER STUDS WHICH ARE DIMENSIONS TO EDGE OF SLAB, MASONRY OPENINGS WHICH ARE FROM FACE OF BRICK, AND AREAS WHERE SHOWN CLEAR DIMENSION IS REQUIRED; U.N.O.	

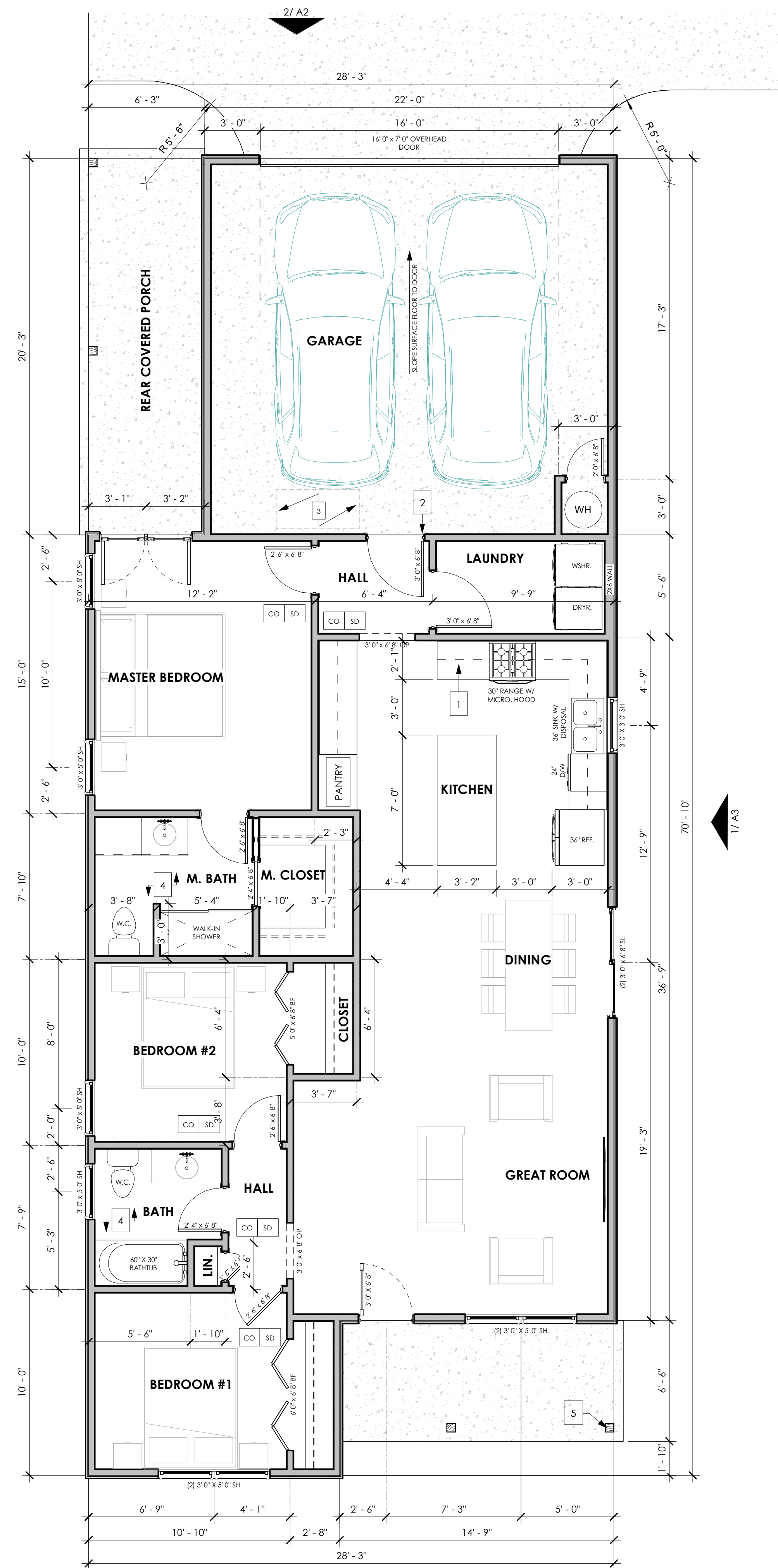
FLOOR PLAN LEGEND		FLOOR PLAN
ITEM	DESCRIPTION	
	NEW WINDOW - FIXED (FX)	
	NEW WINDOW - SINGLE HUNG (SH)	
	NEW DOOR OPENING (OP)	
	NEW ARCHED DOOR OPENING (AO)	
	NEW STANDARD DOOR	
	NEW SLIDING DOOR	
	NEW BI-FOLD DOOR (BD)	
	CEILING MOUNTED SMOKE DETECTOR	
	CEILING MOUNTED CARBON MONOXIDE DETECTOR	

LEGEND NOTES: NOT ALL ITEMS MAY BE USED.

- SMOKE DETECTOR SHOULD BE AT LEAST 6 FT FROM A BEDROOM DOOR.  
- SMOKE DETECTOR SHOULD BE AT LEAST 3 FT FROM A BATHROOM DOOR.
- CARBON MONOXIDE DETECTOR SHOULD BE AT LEAST 10 FT FROM A BEDROOM DOOR.  
- CARBON MONOXIDE DETECTOR SHOULD BE AT LEAST 3 FT FROM A BATHROOM DOOR.

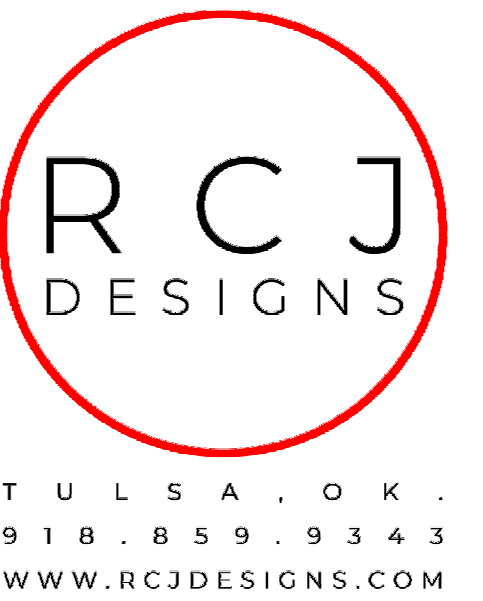
KEYNOTES		FLOOR PLAN
#		
1.	24" CUSTOM COUNTERTOP ON 24" CUSTOM BASE CABINETS, 12" CUSTOM UPPER CABINETS 18" ABOVE COUNTERTOP. BACKSPASH BY OTHERS	
2.	0" - 1 3/4" SOLID WOOD DOOR TO INCLUDE CLOSER WITHOUT DEADBOLT LOCK	
3.	ATTIC ACCESS 25" X 54"	
4.	CELLING MOUNTED EXHAUST FAN TO VENT OUTSIDE FROM THE ROOF	
5.	TYPICAL 6X6 PRESSURE TREATED WOOD POST RE: A5/2	

REFERENCING		FLOOR PLAN
A.	RE: SHEETS A3/A4 FOR EXTERIOR ELEVATIONS	
B.	RE: SHEETS A5 FOR ROOF PLAN	



**1 FLOOR PLAN**  
1/4" = 1'-0"

LIVABLE: 1,306 SQ FT  
GARAGE: 446 SQ FT



DRAWN FOR:  
**TITO PROPERTIES**

**NEW BUILD**

2224 M.L.K. JR BLVD  
TULSA, OK 74106

NO.	REVISION	DATE

PROJECT NUMBER:  
25173

ISSUE:  
PERMIT SET

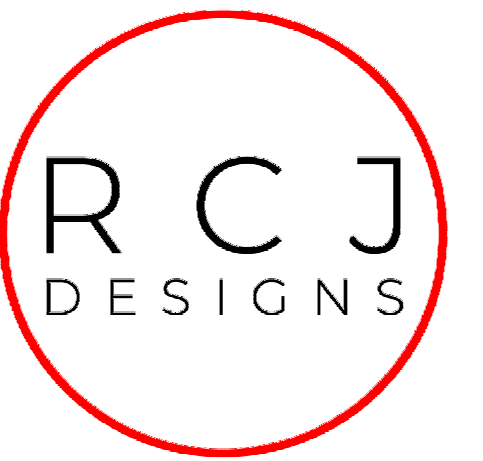
DATE:  
12.17.2025

DRAWN BY:  
MCS

CHECKED BY:  
RCJ

SHEET TITLE:

**FLOOR PLAN**  
**A1**



T U L S A . O K .  
9 1 8 . 8 5 9 . 9 3 4 3  
WWW.RCJDESIGNS.COM



**2** REAR ELEVATION  
1/4" = 1'-0"



**1** FRONT ELEVATION  
1/4" = 1'-0"

DRAWN FOR:  
**TITO PROPERTIES**

**NEW BUILD**

2224 M.L.K. JR BLVD  
TULSA, OK 74106

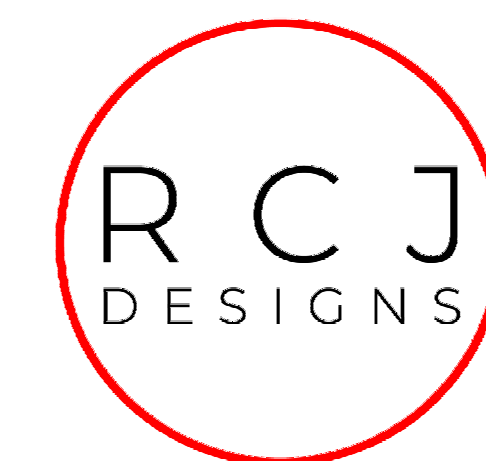
NO.	REVISION	DATE

PROJECT NUMBER:  
25173  
ISSUE:  
PERMIT SET  
DATE:  
12.17.2025  
DRAWN BY:  
MCS  
CHECKED BY:  
RCJ  
SHEET TITLE:

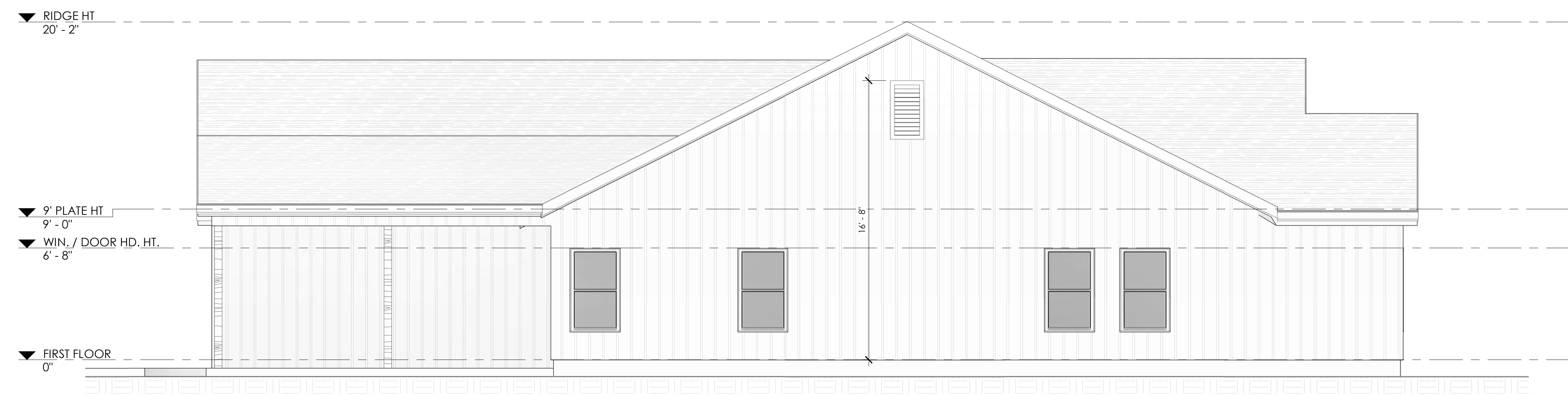
**EXTERIOR ELEVATIONS**

**A2**

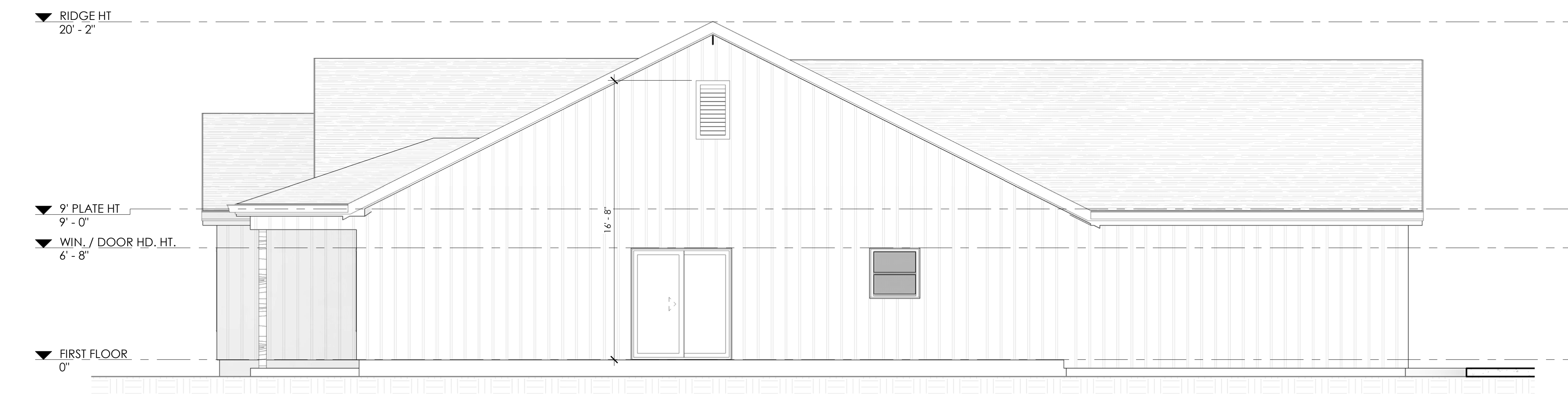
C:\Users\RCJ\OneDrive\Desktop\RCJ Designs\01 RCJ Designs\01 Projects\25173-1to Properties\2224 M.L.K. JR BLVD.dwg



TULSA, OK  
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**2** RIGHT SIDE ELEVATION  
1/4" = 1'-0"



**1** LEFT SIDE ELEVATION  
1/4" = 1'-0"

DRAWN FOR:  
**TITO PROPERTIES**

**NEW BUILD**

2224 M.L.K. JR BLVD  
TULSA, OK 74106

NO.	REVISION	DATE

PROJECT NUMBER:  
25173  
ISSUE:  
PERMIT SET  
DATE:  
12.17.2025  
DRAWN BY:  
MCS  
CHECKED BY:  
RCJ  
SHEET TITLE:

**EXTERIOR ELEVATIONS**

**A3**

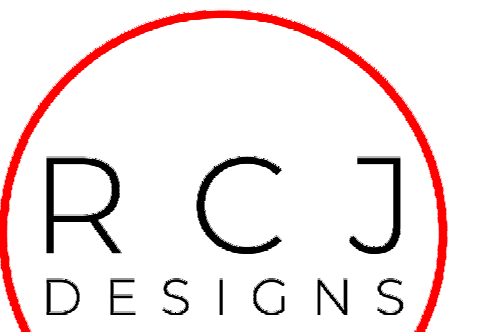
C:\Users\RCJ\OneDrive\Desktop\RCJ Designs\01 RCJ Designs\01 Projects\25173-110 Properties\2224 M.L.K. JR BLVD.dwg

GENERAL NOTES ROOF PLAN

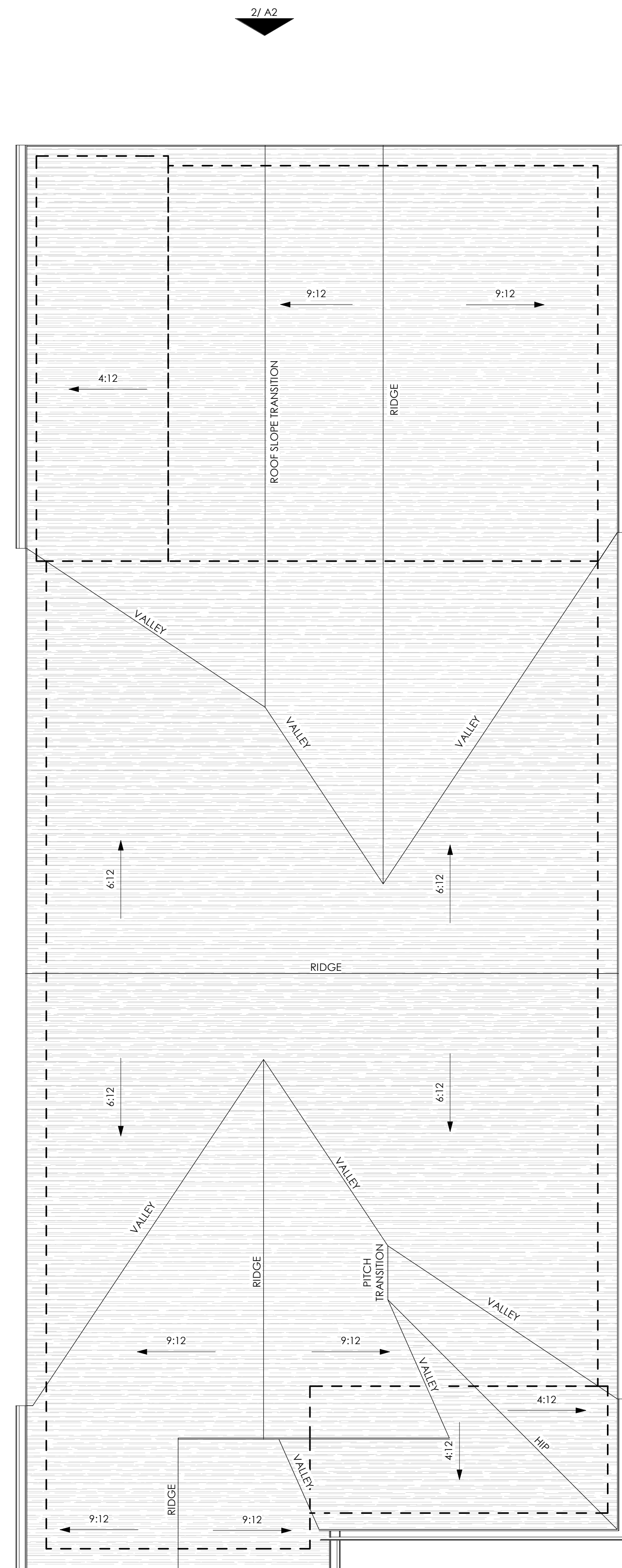
- CONTRACTOR TO PROVIDE ADEQUATE VENTILATION PER R806, IRC 2018

LEGEND ROOF PLAN

- SHINGLE ROOF ASSEMBLY
- NEW FLOOR SLAB OUTLINE



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**1** ROOF PLAN  
1/4" = 1'-0"

DRAWN FOR:  
**TITO PROPERTIES**

**NEW BUILD**

2224 M.L.K. JR BLVD  
TULSA, OK 74106

NO.	REVISION	DATE

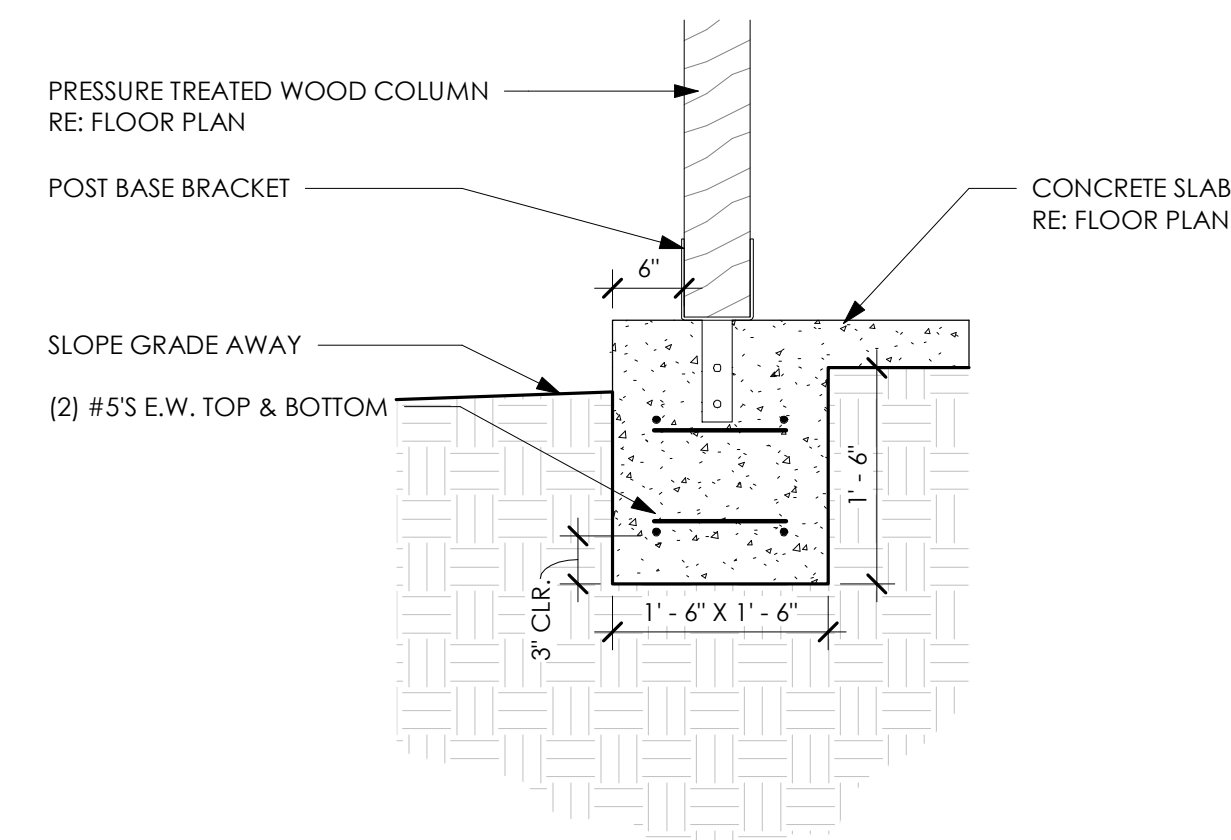
PROJECT NUMBER:  
25173  
ISSUE:  
PERMIT SET  
DATE:  
12.17.2025  
DRAWN BY:  
MCS  
CHECKED BY:  
RCJ  
SHEET TITLE:

ROOF PLAN

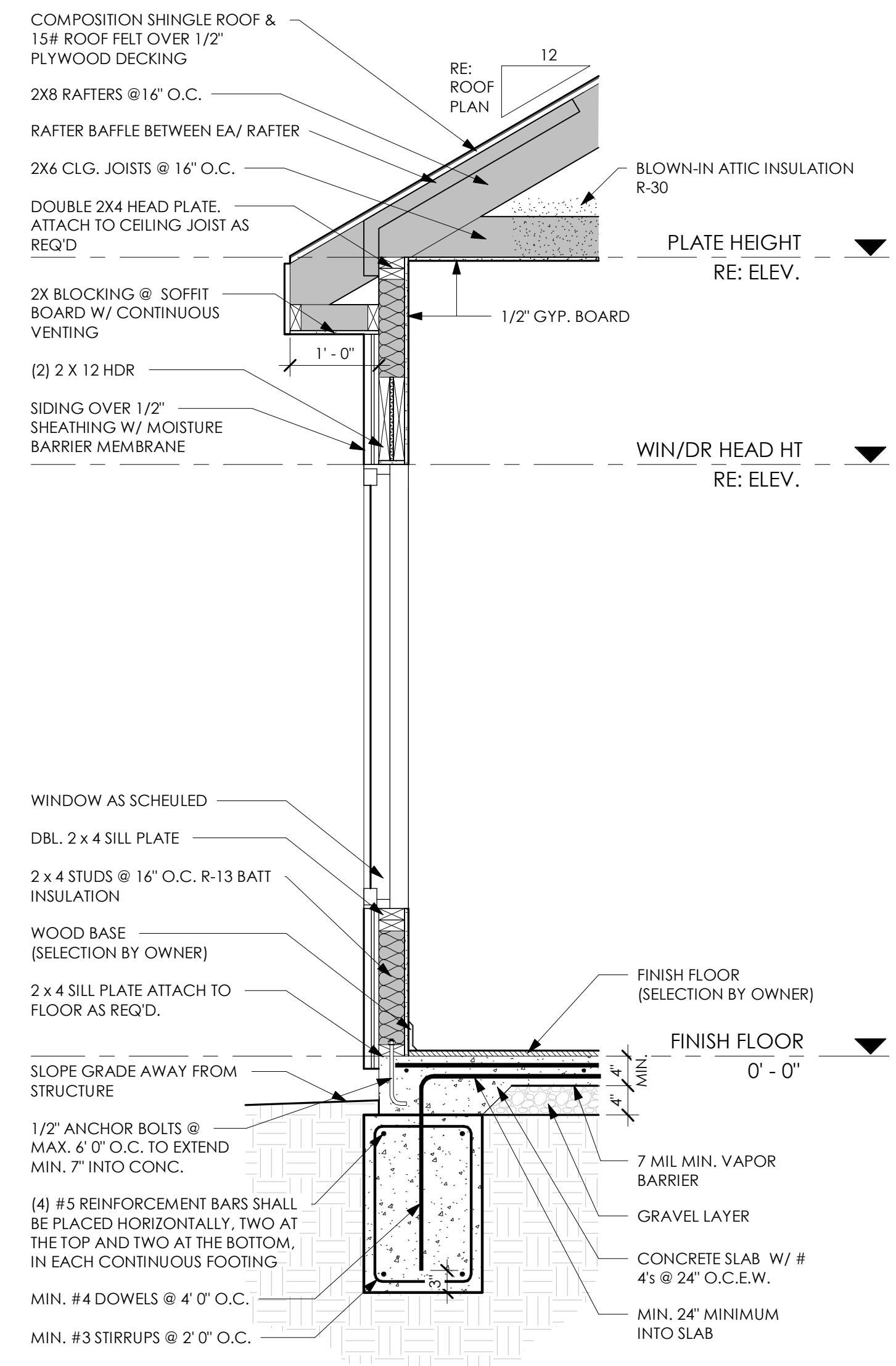
**A4**

- FRAMING NOTES**
- DESIGN AND CONSTRUCTION OF THE ROOF/CILING SYSTEM SHALL ADHERE TO THE PROVISIONS OF CHAPTER 8 OF THE 2018 IRC.
  - ALL FRAMING LUMBER SHALL BEAR A GRADE STAMP OF AN APPROVED GRADING AGENCY.
  - ALL CONNECTIONS USED FOR WOOD CONSTRUCTION SHALL BE SIMPSON STRONG TIE CONNECTIONS OR EQUAL AND INSTALLED PER MANUFACTURER'S INSTRUCTION.
  - ALL FRAME LUMBER SHALL BE DOUGLAS FIR-LARCH GRADE MARKED AS FOLLOWS UNLESS NOTED OTHERWISE ON DRAWINGS:
 

3.1. STUDS	STUD GRADE
3.2. STRUCTURAL JOISTS	NO. 2
3.3. BEAMS AND STRINGERS	NO. 1
3.4. POST AND TIMBERS	NO. 1
  - ALL WOOD IN CONTACT WITH CONCRETE OR MASONRY WHICH IS IN DIRECT CONTACT WITH EARTH SHALL BE PRESSURE TREATED PINE, #2 OR BETTER
  - JOISTS AND RAFTERS SHALL BE BLOCKED AS REQUIRED BY IRC. BLOCKING SHALL BE INSTALLED WITH A TIGHT-FITTING CONNECTIONS AND FREE OF SPLITS
  - ALL TOP PLATES SHALL BE 2 - 2X4 WITH MINIMUM LAP SPLICE OF 48" WITH BUTT JOINTS OCCURRING OVER BEARING STUDS PER CODE
  - ALL JOISTS, HEADERS, BEAMS AND RAFTERS SHALL HAVE A MINIMUM BEARING OF 1 1/2" AT EACH END OF THE SPAN.
  - INSTALL HEADERS OVER OPENINGS IN LOAD BEARING WALLS AS FOLLOWS:
    - OPENINGS UP TO & INCLUDING 4 FEET: 1 - 2X8 OR 2 - 2X6 WITH ONE JACK STUD EACH END.
    - OPENINGS UP TO & INCLUDING 9 FEET: 2 - 2X12 WITH TWO JACK STUDS EACH END.
  - INSTALL 1 - 2X4 TRIMMER OR 4X4 POST UNDER ALL HEADERS AND BEAMS 4" WIDE UNLESS NOTED OTHERWISE ON THE DRAWINGS.
  - INSTALL NOT LESS THAN THREE (3) STUDS AT ALL CORNERS OF EXTERIOR WALLS.
  - ALL JOISTS, BEAMS AND RAFTERS SHALL BE INSTALLED WITH THE NATURAL CROWN UP.



**2** TYP. COLUMN FOOTING DETAIL  
3/4" = 1'-0"



**1** TYP. WALL SECTION  
3/4" = 1'-0"

DRAWN FOR:  
**TITO PROPERTIES**

**NEW BUILD**

2224 M.L.K. JR BLVD  
TULSA, OK 74106

NO.	REVISION	DATE

PROJECT NUMBER:  
25173  
ISSUE:  
PERMIT SET  
DATE:  
12.17.2025  
DRAWN BY:  
MCS  
CHECKED BY:  
RCJ  
SHEET TITLE:

**WALL SECTION/ DETAILS**

**A5**



DEVELOPMENT SERVICES

175 E 2ND ST., STE 405  
TULSA, OK 74103  
918-596-9456

# CITY OF TULSA

## PLAN CORRECTIONS SUMMARY

### BLDR-234514-2025 (2224 MARTIN LUTHER KING JR BLVD Tulsa, OK 74106) Markup Summary #1

WSD (2)



**Subject:** WSD  
**Page Label:** 2  
**Author:** Jeffrey Bush  
**Date:** 12/23/2025 1:09:41 PM  
**Status:**  
**Color:** ■  
**Layer:**  
**Space:**

The drive approach appears to be encroaching onto the neighboring property. This not permitted. Revise plans to ensure radius does NOT encroach onto neighboring property.



**Subject:** WSD  
**Page Label:** 2  
**Author:** Jeffrey Bush  
**Date:** 12/23/2025 1:10:55 PM  
**Status:**  
**Color:** ■  
**Layer:**  
**Space:**

The Site Plan does not show sidewalks on all curb and gutter streets. Sidewalks are required on curb and gutter streets pursuant to:

Title 35 Section 602 B Tulsa City Ordinance  
[https://library.municode.com/ok/tulsa/codes/code\\_of\\_ordinances?nodeId=COOR\\_TIT35INDE\\_CH6FARI-WAC](https://library.municode.com/ok/tulsa/codes/code_of_ordinances?nodeId=COOR_TIT35INDE_CH6FARI-WAC)

Title 42 Section 70.080 B.2. 4 Sidewalks Tulsa Zoning Code  
<http://tulsaplanning.org/programs/zoning/zoning-codes/>

TMAPC Subdivision and Development Regulations Section 5-070 Sidewalks  
<http://tulsaplanning.org/plans/Tulsa-Subdivision-Development-Regulations.pdf>

Revise site plan to show public sidewalks on all curb and gutter streets. Sidewalks must be shown on the site plan for permit approval, even if you intend to pay the fee-in-lieu of sidewalks at a later date.

10-18



DEVELOPMENT SERVICES

175 E 2ND ST., STE 405  
TULSA, OK 74103  
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# CITY OF TULSA

## PLAN CORRECTIONS SUMMARY

### Zoning Review (1)



**Subject:** Zoning Review  
**Page Label:** 2  
**Author:** DWhiteman  
**Date:** 12/30/2025 9:45:42 AM  
**Status:**  
**Color:** ■  
**Layer:**  
**Space:**

Section 90.090-A.1, Measurement, Setbacks. Street setbacks are measured from the actual right-of-way line of the street (other than an alley), provided that if the following measurement results in a greater set-back, the greater setback applies: For streets shown on the major street and highway plan, if the width of the planned right-of-way exceeds the width of the actual right-of-way, the measurement must be taken from the planned right-of-way.

REVIEW COMMENT: According to the Major Street and Highway Plan, at this location the planned right of way (ROW) width for N. Martin Luther King Jr. Blvd. is 100'. It is a Secondary Arterial street. RS-3 zoned lots require a front street setback of 35' from an arterial street, and at this location the setback measurement starts 50' from the center of the ROW, for a total of 85' from the center of N. Martin Luther King Jr. Blvd. Please revise the plans to show a setback of 85' from the center of the right of way. Otherwise you may request a variance from the Board of Adjustment to reduce the required setback from the planned right of way of an arterial street.